
21. *Corridor commercial (CCM) district.*

- a. The purpose of the corridor commercial CCM zoning district is to provide regulations and graphic design standards to enhance both the function and appearance of the U.S. Route 13 Corridor through the implementation of strategies outlined in both the Smyrna U.S. 13 Corridor Plan and Design Book and the Town of Smyrna Comprehensive Plan.
- b. *Purpose.*
- (1) Construct buildings close to the roadway that possess an architectural form and theme reflective of the character of Smyrna's Historic District.
 - (2) Provide retail and service establishments that supply commodities or perform services that will meet the needs of adjacent residential neighborhoods as well as the needs of travelers and visitors.
 - (3) Allow greater diversity of housing choices by permitting residential uses located directly above first floor retail or office space.
 - (4) Create convenient and controlled vehicular access onto U.S. Route 13, reducing the need for additional curb cuts and promoting interconnectivity between lots.
 - (5) Establish off-street parking located at the rear or side of the buildings, reducing the visual impact of parking lots.
 - (6) Encourage pedestrian and multi-modal connectivity and safety by requiring sidewalks, crosswalks, streetlights, walking and bike trails, and bike racks.
 - (7) Promote "complete" site design through various design standards, such as landscaping, decorative elements, interconnectivity and open space requirements.
 - (8) Reduce the visual clutter of various types of existing signage along the corridor and implement consistent standards regulating the height, placement, lighting and materials.
 - (9) Provide a variety of functional open space and recreational amenities to serve and compliment the commercial, office, and residential uses within a project and in adjacent areas.
 - (10) Create a vibrant, uniform, safe, and attractive streetscape along the U.S. Route 13 corridor through the planting and installation of street trees, wide sidewalks, streetlights, appropriately-scaled signage, and a mix of buildings and uses that mimic that of a downtown commercial district.

Illustrations included in this document are examples of how the intent of the required standards can be achieved.

- c. *Applicability.* From and after the date of the final adoption of this corridor commercial (CCM) zoning district, any building, structure, premise, parcel, sign or other facility located wholly or partially in the corridor commercial district (1) being in any manner erected, constructed, built, converted, created, reconstructed, modified, moved, altered, added to or converted; or for any change in the exterior architectural appearance of any structure by additions, alteration, or replacement; and (2) which requires a site plan, subdivision, building permit, or demolition permit shall conform with the provisions pertaining to this district.

Anything to the contrary notwithstanding, the provisions contained herein shall not apply to any of the following: (1) additions to a building cumulatively totaling ten percent or less of the total square footage of the existing building(s) as of the date of adoption of this ordinance; (2) interior only renovations and rehabilitation projects that require a building permit; (3) the construction of accessory structures that are smaller than the lesser of 1,500 square feet or 50 percent of the gross floor area of

the commercial use areas of the principal building; (4) minor projects that do not require a building permit or demolition permit or (5) ordinary exterior maintenance and repairs to prevent or correct wear or damage to a structure, or any part thereof, including deterioration of exterior walls or siding materials, columns, posts or other vertical supports; roofs, cornices soffits or fascia; exterior chimneys; exterior plaster, mortar, or ornamental trim; doors, windows, shutters, porches, railings or any other exterior features; or exterior paint due to extended lack of maintenance and upkeep.

- (1) *Existing conditions.* U.S. Route 13, from Duck Creek north to the S.R. 1 interchange currently functions as a high-speed arterial with a posted speed limit of 55 mph. This area is lined with a mixture of suburban-style commercial buildings, mid-20th century-style houses and vacant land, with the majority of buildings set back from the road. Sidewalks, bike paths and pedestrian scale improvements are not present; signage with no consistent design style appears cluttered and various curb cuts for property access gives the impression that the corridor is built for thru-traffic only and lacks character and purpose.

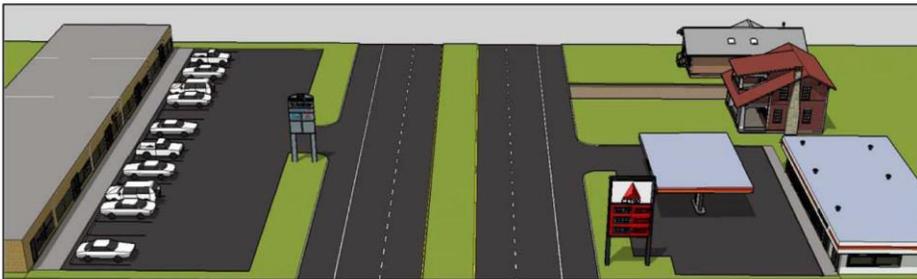


Illustration 1 - Existing, undesired conditions

- (2) *Required conditions.* The corridor will continue to serve as a regional transportation route, but it is envisioned to become a more attractive urban-style boulevard. Two to three story buildings will be encouraged, with all buildings required to be constructed close to a ten-foot-wide paved sidewalk adjacent to a five-foot-wide grass buffer located in the existing right-of-way on both sides of the roadway. Streetlights will be installed at regular intervals to create a more pedestrian-friendly environment.

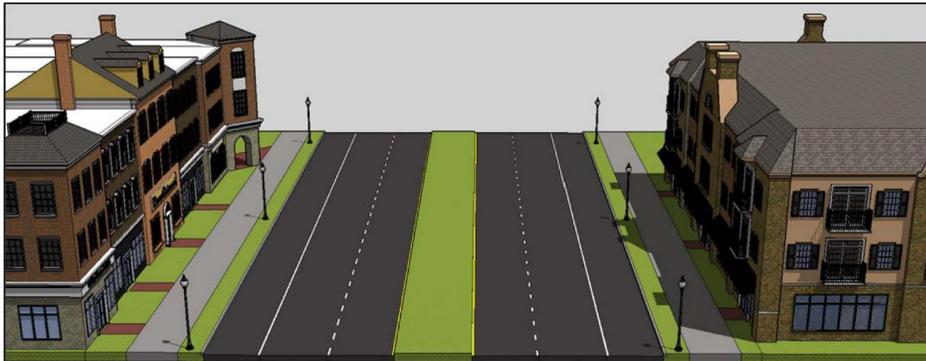


Illustration 2 - Preferred Conditions

- d. *Building setback standards.*

- (1) *Intent.* Buildings constructed along U.S. Route 13 are to be located within close proximity to the sidewalk and in general alignment with adjacent buildings in a manner that is reflective of Smyrna's historic downtown to ensure the visibility of retail businesses and encourage active, lively uses. Buildings, along with trees and landscaping shall be predominant rather than parking lots.



Illustration 3 - Front Yard Setback Example

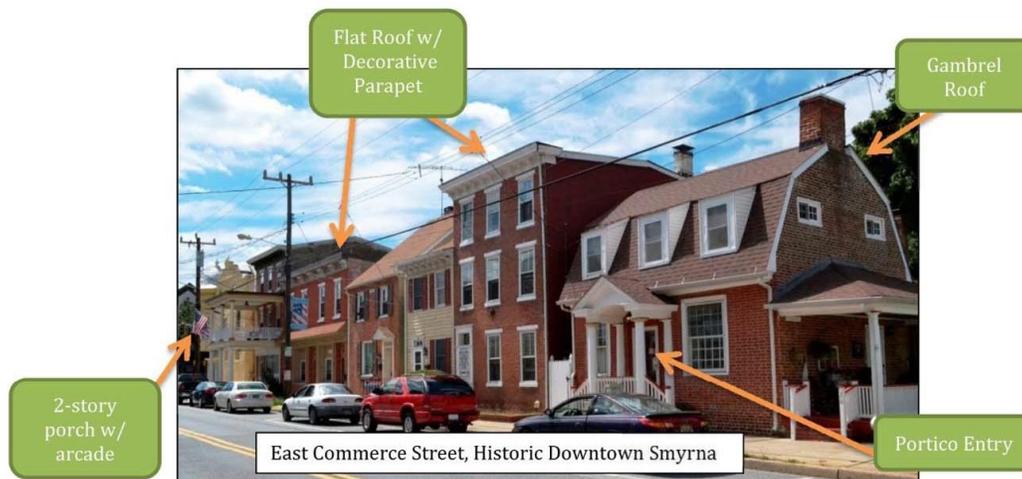
- (2) *Minimum lot requirements.*
- (a) Minimum lot width: 50 feet.
 - (b) Minimum lot depth: 100 feet.
- (3) *Setback standards.*
- (a) Front yard setback:
 - i. Minimum: five feet.
 - ii. Maximum: 15 feet.
 - (b) Rear yard setback:
 - i. Minimum: zero feet, except a minimum of 20 feet shall be provided to include a ten-foot planted buffer when adjacent to an existing residential use.
 - ii. Maximum: N/A
 - (c) Side yard setback:
 - i. Minimum: Zero feet, except a minimum of 20 feet shall be provided to include a ten-foot planted buffer when adjacent to an existing residential district.
 - ii. Maximum: N/A
 - (d) The front yard setback will be calculated from the property line fronting U.S. Route 13 or, when a building is not adjacent to U.S. Route 13 but is instead adjacent to a public or private internal street situated on the property, from the outside of the adjacent sidewalk.

Rear-access alleys and streets shall not be considered internal streets for the purposes of meeting the front setback. Where a building or site has multiple street frontages, the front yard shall be measured from U.S. Route 13 or, where that is not reasonably practical, from the side of the sidewalk abutting the adjacent street that directly accesses U.S. Route 13. The planning commission may waive a maximum front yard setback for a building directly fronting a green, plaza, or other open space.

e. *Building height/massing standards.*

- (1) *Intent.* To reduce the apparent bulk of buildings and to maintain pedestrian scale, a sense of "base" and "top" shall be provided. Building facades shall have a distinct "base" using articulation and materials that reinforce the character of the streetscape by encouraging the greatest amount of visual interest along the ground floor, street facing facades of buildings. The roofline or "top" of a building shall emphasize a distinct profile or outline. The "middle" of the building is encouraged to be made distinct by a change in material or colors, windows, balconies, setbacks and signage.

The construction of two to three story buildings is strongly encouraged to emulate the traditional character of Smyrna's historic downtown, however single-story buildings are permitted, provided that they have a minimum height of 16 feet with a decorative parapet, gabled, hipped, mansard, or gambrel roof. (See definitions.)



- (2) *Building height standards.* Building height shall be defined as the vertical distance from the finished grade to the top of the highest roof beams on a flat roof, to the deck level on a mansard roof, and the average distance between the eaves and the ridge level for gable, hipped and gambrel roofs.

(a) Height:

- i. Minimum: 16 feet.
- ii. Maximum: 60 feet, maximum four stories.



Illustration 4 - Building Height - Flat Roof

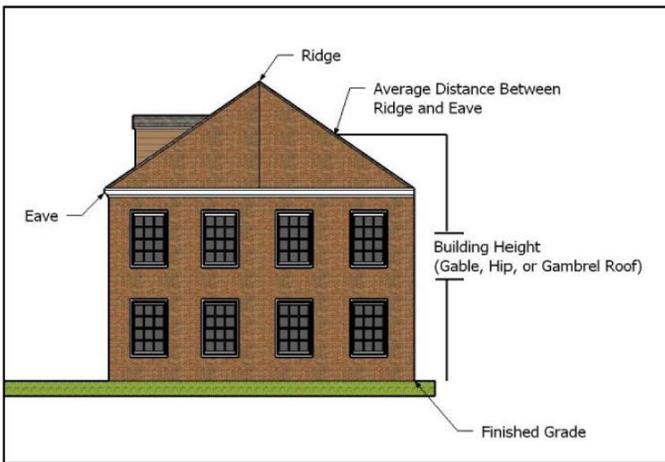


Illustration 5 - Building Height - Gable, Hipped, or Gambrel Roof



Illustration 6 - Building Height - Mansard Roof

- (b) Stories. Multi-story buildings are strongly preferred throughout the CCM district and especially fronting on U.S. Route 13. One-story buildings shall utilize raised parapets, gable

and gambrel style raised roofs, faux second- or third-story windows, and/or other decorative features to replicate the scale and character of traditional downtown multi-story commercial and mixed-use buildings.



Illustration - New one-story commercial building in Chestertown, MD made to emulate a diverse 1½ to 2-story downtown storefront



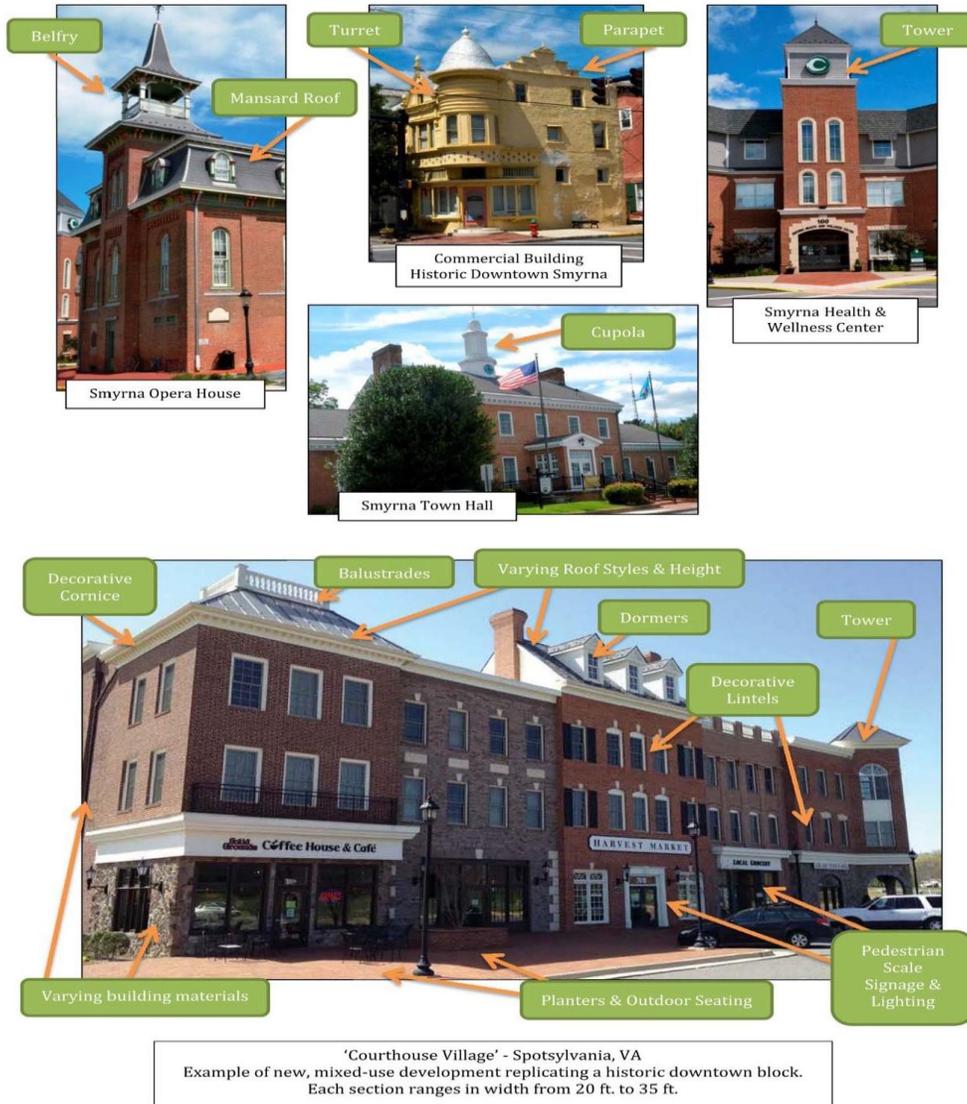
Illustration - New one-story CVS in Eagle, PA that utilized numerous architectural embellishments to resemble a two-story commercial building

- (3) *Facade "base" standards.* Ground floor facades facing U.S. Route 13 shall incorporate at least five of the following elements: (Note: Ground floor facades facing a public street other than U.S. Route 13 or common amenity space shall incorporate at least three of the following elements:)
- (a) Translucent windows.
 - (b) Arcades.
 - (c) Porches, porticos.
 - (d) Marquees.
 - (e) Decorative belt courses.
 - (f) Canopies, awnings.

- (g) Decorative window lintels.
- (h) Decorative lighting.
- (i) Pilasters or columns.
- (j) Any standard not listed, as approved by the planning and zoning commission.



- (4) *Roofline "top" guidelines.* At least two of the following elements are required:
- (a) Varying roof styles and heights.
 - (b) Dormers.
 - (c) Chimneys.
 - (d) Cupolas.
 - (e) Belfries.
 - (f) Balustrades.
 - (g) Decorative cornices.
 - (h) Any standard not listed, as approved by the planning and zoning commission.



f. *Facade articulation standards.*

- (1) *Intent.* Buildings shall provide diversity and variation in facade type, width, and roof heights and types to reinforce the character of the streetscape by encouraging visual interest along the ground level of buildings. The facade of large single-use buildings like grocery stores, big box stores, and office buildings shall be broken up to mimic a series of smaller storefront buildings in the historic downtown of Smyrna.



Illustration 7 - Façade Articulation Standards Example

- (2) Buildings with a length of at least 60 feet or greater shall provide at least three of the following architectural façade treatments:
- (a) Variation in building materials.
 - (b) Porches, portico, canopies, etc.
 - (c) Translucent windows.
 - (d) Wall offsets.
 - (e) Varying roof types or heights.
 - (f) An architectural element not listed above, as approved by the planning and zoning commission.





Illustration - Grocery store with architectural improvements to break up the facade of this 60,000 square feet into smaller sections as may be found in an historic downtown commercial district.

g. Window standards.

- (1) *Intent.* Windows shall be provided on the ground floor facade of all buildings facing U.S. Route 13 and all other internal public or private streets (except rear-access alleys). Apartment buildings and townhouses shall be exempt from these standards.
- (2) *Standards:*
 - (a) Forty percent of the area of all ground floor building facades situated higher than two feet and lower than ten feet above ground level fronting U.S. Route 13 and other public and private streets (except rear-access alleys) shall consist of windows.
 - (b) Sills of the windows shall be no higher than three feet above grade.
 - (c) Windows on front facades facing a street shall be transparent or translucent glass.
 - (d) The use of opaque glass, such as tinted, smoked, or mirrored glass is not permitted.



Illustration 8 - Window Standards Example

h. Streetscape design standards.

- (1) *Intent.* Streetscape design shall ensure that the street edge provides a transition between the roadway, the sidewalk and buildings. Buildings, along with sidewalks, trees, and landscaping shall

be predominant rather than parking lots. Street wall/buffer standards shall screen parking lots, driveways and service station fuel pumps. Site furnishings such as benches and trash receptacles compliment the streetscape and create a more pedestrian-friendly atmosphere.

(2) *Sidewalk standards.*

- (a) Sidewalks along U.S. Route 13 shall be concrete and ten feet in width and separated from the shoulder of U.S. Route 13 by a grass area five feet in width.
- (b) Sidewalks along all other roadways shall be five feet in width and separated from the shoulder of the roadway by a grass area five feet in width.

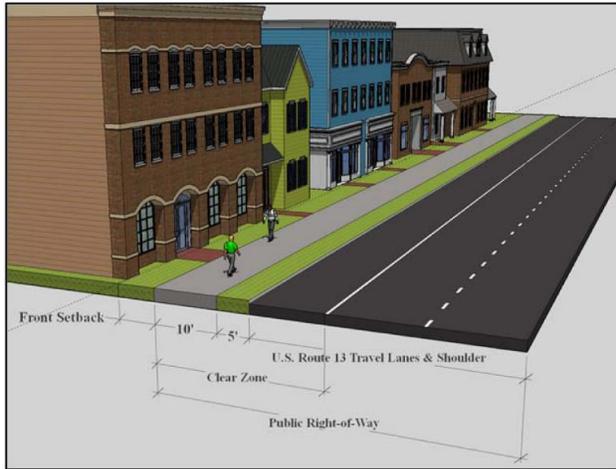


Illustration 9 - Sidewalk - U.S. Route 13



Illustration 10 - Sidewalks - All other Roadways

(3) *Street wall/buffer standards.*

- (a) A street wall or buffer shall be required to be installed and maintained wherever a parking lot, driveway, drive-thru, or service station fuel pumps directly front a public or private street (except rear-access alleys) within this district. The purpose and intent are to create an attractive streetscape and shield and minimize auto-related uses from public view.

- (b) Street wall/buffers shall not be located in the public right-of-way and shall be located along the outside of the sidewalk beyond the clear zone as defined by DeIDOT.
- (c) Street wall/buffers shall be permanently maintained by the property owner and/or development association.
- (d) The preferred street wall or buffer consists of brick piers two feet square with a maximum height of three and one-half to four feet tall, constructed at intervals of 20 feet with a connecting black iron, steel, aluminum (or approved alternative) fencing constructed to a height of two and one-half to three feet. Fence openings shall have a spacing between four and eight inches. Low-lying shrubs are encouraged to be planted at the base of said street buffer and shall not exceed a maximum height of three feet at maturity. This preferred street wall/buffer is outlined below in illustrations 11 and 12 below.

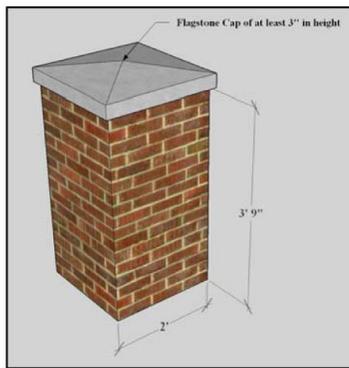


Illustration 11 - Example of Preferred Brick Pier

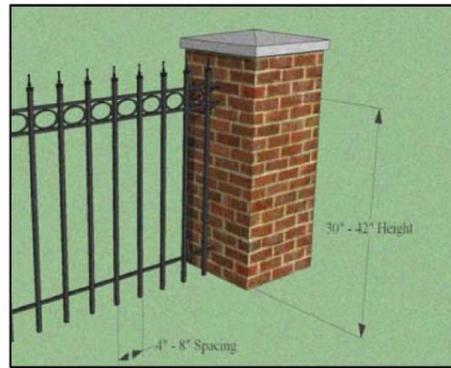
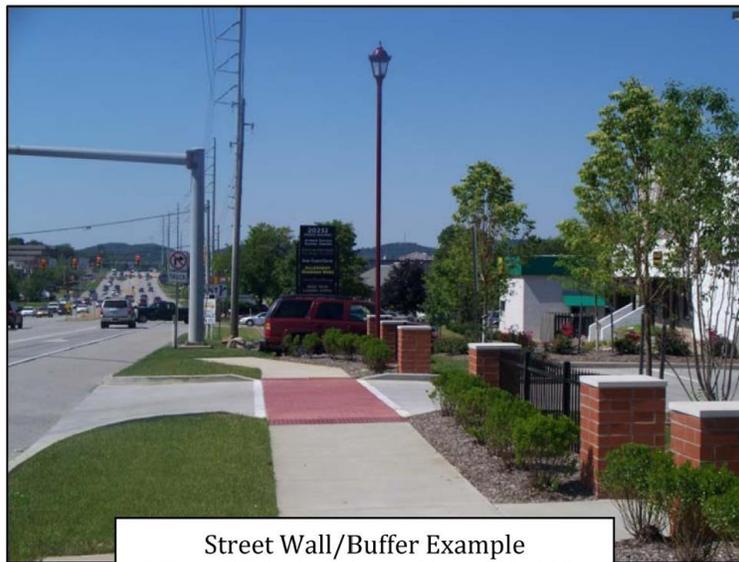


Illustration 12 - Example of Preferred Street Wall/Buffer Fence Detail



**Street Wall/Buffer Example
Route 19 in Cranberry Township, PA**

- (e) An acceptable alternative to a street wall/buffer is to plant a continuous evergreen hedge between three and four feet in height as outlined in the illustration below.



Illustration - Continuous vegetative hedge screening a financial institution's parking lot along Baltimore Pike (U.S. Route 1) in Media, PA

- (f) An acceptable alternative to a street wall/buffer is to install a continuous brick or stone masonry wall between three and four feet in height as outlined in the illustration below. Low lying shrubs and/or flower bedding are encouraged to be planted at the base of said masonry wall facing the sidewalk.



Illustration - Continuous masonry wall with attractive shrubbery and flower bedding at the wall's base in Chestertown, MD

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- (g) The planning commission may approve an alternative street wall/buffer not listed here so long as it meets the intent of this section.
- (4) *Street tree standards.*
- (a) One street tree shall be planted every 30 linear feet of frontage along U.S. Route 13 and along all internal public and private streets.
- (b) Street trees shall be planted between the curb and the sidewalk where practical, and not within 60 feet of an intersection.
- (c) At the time of planting, street trees shall be no less than two inches in caliper.
- (d) No more than three street trees of the same species may be planted in a row.
- (e) The following street trees may be planted:
- Ginkgo biloba 'Princeton Sentry' (male no fruit).
 - Shawnee Brave Bald cypress (Taxodium distichum 'Mickelson').
 - Hardy rubber tree (Eucommia ulmoides).
 - Freeman maple (Acer x freemanii 'Scarsen') Scarlet Sentinel.
 - Thornless Honeylocust (Gleditsia triacanthos var. inermis 'Shademaster').
 - American Hornbeam (Carpinus caroliniana).
 - New Horizon Elm (Ulmus 'New Horizon').
 - Hornbeam (Carpinus betulus 'Fastigiata').
 - Eastern Hop Hornbeam (Ostrya virginiana).
 - Trident Maple (Acer buergeriaman).
 - Miyabei Maple (Acer miyabei 'Morton' State Street).
- (f) Alternative street trees that meet the intent of this zoning district may be approved on a case-by-case basis by the Smyrna Shade Tree Commission.
- (5) *Streetlights.*
- (a) To ensure pedestrian lighting and safety, streetlights shall be installed every 60 linear feet, but no closer than 15 feet from any street tree, along all public and private streets (except rear-access alleys)
- (b) Streetlights shall be a Sternberg Lighting Model Town Square A880SRLED as shown below or a close alternative approved by the planning commission.



Sternberg Lighting Model Town Square A880SRLED with Sternberg Lighting Model Town Square A880SRLED with a single acorn-shaped luminaire **Sternberg Lighting Model Town Square A880SRLED with dual acorn-shaped luminaires**

- i. *Building placement (drive-thru buildings and service stations).*
 - (1) *Intent—Drive-thru buildings.* Drive-thru buildings contain a drive-thru as a part of the commercial or service establishment. Typical uses include banks, pharmacies and fast-food establishments.
 - (2) *Standards—Drive-thru buildings.*
 - (a) Drive-thru buildings shall not locate drive-thru windows on those facades directly facing U.S. Route 13.
 - (b) Drive-thru buildings located on a corner lot shall provide a separate entrance along U.S. Route 13 and exit along the side street.



Illustration 13 - Drive-thru Building, Corner Lot - Example

- (c) It is strongly preferred that drive-thru buildings provide a separate entrance and exit with one-way automobile circulation as shown below.



- (d) Drive-thru buildings may be configured with a drive aisle positioned in the front yard (as shown below) if the following criteria are met: it is impractical to provide separate entrances and exits to the facility, the building is located no farther than 25 feet from the front property line, and the applicant obtains a waiver from the planning commission.



Illustration 14 - Drive-thru Building, Entrance - Example

- (3) *Intent—Service stations.* Service stations feature one or more islands of fuel pumps and a convenience store. The intent of this section is to minimize the road frontage visibility of fuel pumps and maximize the visibility of the convenience store.
- (4) *Standards—Service stations.*

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- (a) Fuel pumps are strongly encouraged to be located to the rear of the convenience store, shielded from frontage along U.S. Route 13.



Illustration 15 - Fuel Pumps, Rear Location - Preferred Example

- (b) Fuel pumps may be located to the side of the convenience store. Fuel pumps shall not be located in the front yard of those parcels fronting U.S. Route 13.



Illustration 16 - Fuel Pumps, Side Yard Location - Acceptable Example

- (c) Fuel pump canopies shall be constructed utilizing the same building materials used in the construction of the associated convenience store.



(d) Fuel pumps shall not be located closer than 25 feet to the public right-of-way along U.S. Route 13.

j. *Parking lot design.*

(1) *Intent.* A streetscape formed by a continuous row of buildings located close to the sidewalk with minimal parking lot frontage along U.S. Route 13 will create an attractive urban-boulevard, reflective of the town's historic downtown. Parking lots shall be landscaped to provide an aesthetically pleasing streetscape.



(2) *Standards.*

- (a) Parking lots shall be designed to the maximum extent feasible to connect parking lots on adjacent lots by providing cross-property easements to share driveways and reduce the need for additional curb cuts. Future connections for parking lot interconnectivity between adjacent lots shall be required.
- (b) In the case of a driveway located between parking rows adjacent to buildings, a minimum of 18 feet shall be provided for emergency vehicle access.
- (c) Access to fire connections shall be provided for.
- (d) Parking lot speed bumps shall be prohibited.

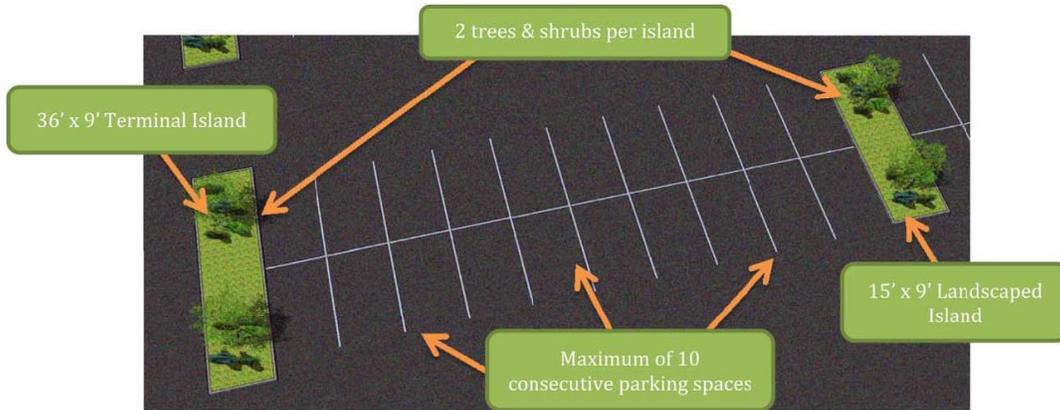
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- (e) Bicycle racks shall be provided and shall be located in well-lit parking areas, safe from vehicular traffic. A minimum of one rack with five bicycle parking spaces shall be provided in all parking lots containing 20 or more parking spaces. Bicycle racks shall be black in color.
 - (f) Parking is not permitted in the front of a building. It is preferred that parking be located to the rear of a building to reduce the visual impact of the parking lot, though parking to the side of a building is acceptable.



- (g) Commercial buildings are encouraged to share a common entrance from the roadway to their respective parking lots that are located to the rear of each building.



- (h) A parallel parking row shall be limited to 15 consecutive parking spaces interrupted by a 15-foot by nine-foot landscaped island to separate adjacent parking spaces. Two trees and two low-lying shrubs shall be required for each landscaped island.

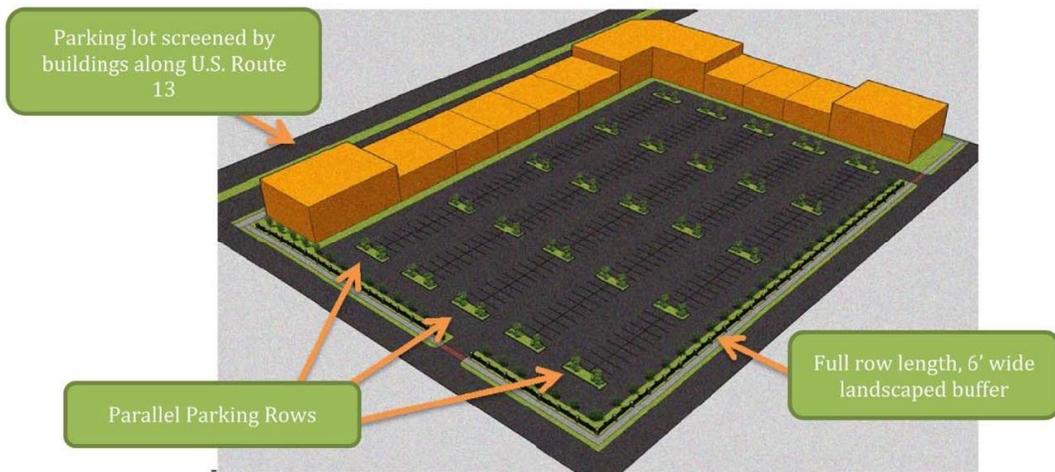


- (i) All parking rows shall be terminated with a 36-foot by nine-foot landscaped, terminal island to protect parked vehicles and confine moving traffic to aisles and driveways. Two trees and two low-lying shrubs shall be required per terminal island.

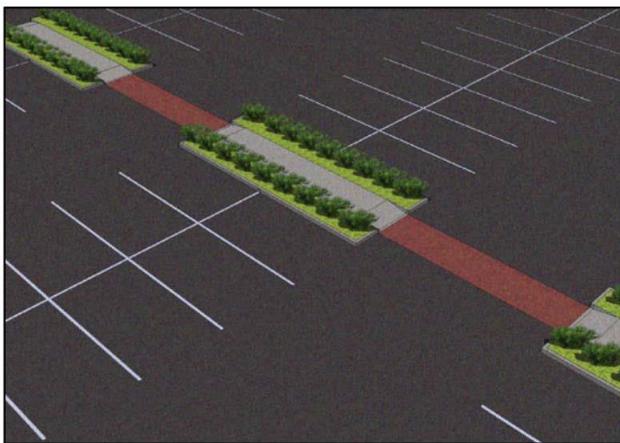


- (j) Parking lot trees shall be pruned so that branches do not obstruct the light from any street lamp or obstruct the view of any street intersection. Trees shall also be pruned to a minimum height of 14 feet to provide a clear space above surface sidewalks, parking lots and driveways to allow for unobstructed passage of pedestrians and vehicles.
- (k) Parking lot lights shall be installed and maintained in all off-street parking lots to ensure pedestrian lighting and safety.

- (l) All parking lot lights shall have one or two downward facing decorative u-shaped lamps, including a round pole shaft with a round cast base in a black, powder coated finish. Cobra headed light pole styles are prohibited. An alternative parking lot light style may be approved by the planning commission.
- (m) A maximum of five consecutive parallel parking rows are permitted prior to a full row length landscaped area a minimum of six feet wide.



- (n) A minimum six-foot wide, full row length landscaped buffer shall be installed between all parking rows abutting adjacent properties and secondary access roads. One tree shall be planted for every 30 linear feet of frontage. An exception may be made if the parking lot is designed to create interconnectivity through shared parking for adjacent lots. Landscaping shall be permanently maintained by the property owner and/or development association.



- (o) Parking lots with more than five parallel parking rows shall construct sidewalks and crosswalks through landscaped islands bisecting the parking lot at or near the midpoint to increase pedestrian safety and connectivity. Developers may also submit an alternative design subject to planning and zoning commission approval.
- (3) *Modified parking lot design.* To minimize the overall footprint of parking lots, the construction of a modified parking design is encouraged.

Property owners, land developers, or land users of any parcels in the CCM (corridor commercial) zoning district may install off-street spaces with modified designs with planning commission approval.

Modified parking designs include, but are not limited to:

- (a) Double stacking residential parking spaces for each residential unit.



- (b) Placing parking spaces on the ground story of a building, but behind retail/office uses or a facade wall.

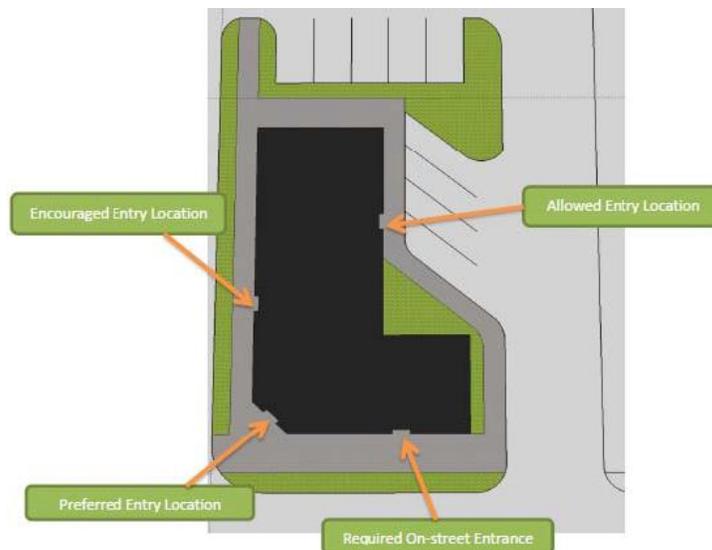


- (c) Multi-story structured parking garages are permitted so long as they are located behind retail/office uses or buildings or behind a facade wall designed to replicate a multi-story commercial building. Structured parking garages may not directly front U.S. Route 13 but may front an internal public or private side street.



k. *Building entrances.*

- (1) Intent. Buildings are encouraged to be oriented towards U.S. Route 13 or internal commercial boulevards, and thus provide entrances that are accessible to and in sight of the roadway and sidewalk. Entrances are encouraged to be prominently displayed and include architectural embellishments so as to highlight their location(s) along the building.
- (2) All buildings along U.S. Route 13 and internal commercial boulevards shall include entrances facing the roadway along the ground story.
- (3) All buildings and/or tenant space must have clearly defined, highly visible customer entrances, easily identifiable from the street and sidewalk.
- (4) Walls facing side streets and/or access roads must have windows and are strongly encouraged to have doors.
- (5) Corner buildings and end units are strongly encouraged to place their primary entrance at the corner of the intersection and a secondary entrance along the side street.



I. *Common amenity space.*

- (1) *Intent.* Commercial and mixed-use development in the corridor commercial (CCM) zoning district shall provide a variety of useable common amenity spaces and amenities that are integrated into the overall site design and plan that enhances the character of U.S. Route 13. The open space requirements outlined in section 5.07(b)(5) of the Subdivision and Land Development Ordinance shall not apply to the CCM district, which district shall instead be bound by the common amenity space standards set forth herein.
- (2) *Standards.*
 - (a) Non-residential projects shall be required to provide at least one of the following amenities for the first 10,000 square feet of gross floor area and one additional amenity for every 25,000 square feet of gross floor area thereafter rounded to the nearest increment of 25,000 square feet. Residential projects shall provide at least one of the following amenities for every 50 dwelling units, rounded to the nearest increment of 50 dwelling units. The number of amenities required for mixed-use projects shall be calculated based on the collective number of amenities required for each of the residential and non-residential portions based on the calculations herein provided.
 - (b) Common amenity space shall be visible and accessible from a public sidewalk. It is preferred that such space be in close proximity to the main entrance of a building to take advantage of pedestrian traffic, but other locations may be considered if they are visible and accessible. Common amenities shall be ADA accessible unless an exemption is lawfully granted pursuant to all applicable laws and regulations.
 - (c) Common amenity space shall be provided in one or more of the following forms, based on the calculations set forth in subsection (a):
 - (i) *Pocket parks.* Pocket parks shall be incorporated into the site design at building entrances, street corners, and/or sidewalks and shall range in size from 500 square feet to 1,000 square feet. Two pocket parks of the before-mentioned size and with the below features shall be counted as the equivalent of one common amenity space.

To enhance the attractiveness of the overall site design and to encourage pedestrian connectivity between buildings, parking lots and adjacent commercial uses, pocket parks shall feature at least three different elements from the following list:

- (a) Sitting areas (such as benches, low seating walls).
- (b) Covered structure (such as a gazebo, pergola or pavilion).
- (c) Decorative paving (such as colored/stained concrete, brick or other type of paver).
- (d) Decorative planters.
- (e) Decorative flowering or evergreen shrubs.
- (f) Decorative, pedestrian-scale lighting.
- (g) Public art (such as monuments, sculptures, statuary).
- (h) Decorative water feature (such as a fountain, rain garden).



This 800 sq. ft. Pocket Park breaks up long stretches of sidewalks. It also serves to connect sidewalks and crosswalks within larger parking lots to the public sidewalk.



This Pocket Park is located at the edge of a large parking lot to connect the crosswalk with the adjoining sidewalk. The covered structure provides shelter from the elements and compliments the overall attractiveness of the site.

- (ii) *Public squares or plazas.* Public squares and plazas shall be incorporated into the overall site design so that primary building facade(s) front the respective public square or plaza and shall range in size from 2,500 square feet to 10,000 square feet.

To enhance the attractiveness of the overall site design and to encourage pedestrian connectivity between buildings, parking lots and adjacent commercial uses, public squares or plazas shall feature at least five different elements from the following list:

- (a) Sitting areas (such as benches, low seating walls).
- (b) Covered structure (such as a gazebo, pergola or pavilion).
- (c) Decorative paving (such as colored/stained concrete, brick or other type of paver).
- (d) Decorative planters.
- (e) Decorative flowering or evergreen shrubs.
- (f) Decorative, pedestrian-scale lighting.
- (g) Public art (such as monuments, sculptures, statuary).
- (h) Decorative water feature (such as a fountain, rain garden).



This Public Square features a centrally located water fountain surrounded by benches, shrubbery, and shade trees with primary building facades fronting the square. Note the adjacent on-street dining.



This Public Plaza features a central water fountain surrounded by benches, decorative shade trees and landscaping with storefronts facing the square.

- (iii) *Town center greens.* Town center greens shall be generally rectangular in shape and shall be allowed on parcels greater than five acres in size. They shall be not be permitted to be located directly on the corner of intersections along U.S. Route 13.

Town center greens shall range in size from 10,000 square feet to 45,000 square feet. Any design in excess of 45,000 square feet may be approved by the planning and zoning commission if it meets the intent of these standards.

Primary building facades shall directly front onto the town center green on at least three sides and are strongly encouraged to front the green on all four sides.

To enhance the attractiveness of the overall site design and to encourage pedestrian connectivity between buildings, parking lots and adjacent commercial uses. Town center greens shall feature at least six different elements from the following list:

- (a) Sitting areas (such as benches, low seating walls).

- (b) Covered structure (such as a gazebo, pergola or pavilion).
- (c) Decorative paving (such as colored/stained concrete, brick or other type of paver).
- (d) Decorative planters.
- (e) Decorative flowering or evergreen shrubs.
- (f) Decorative, pedestrian-scale lighting.
- (g) Public art (such as monuments, sculptures, statuary).
- (h) Decorative water feature (such as a fountain, rain garden).



This 125' x 350' (43,000 sq. ft.) green features a water fountain and gazebo and is fronted by multi-story mixed-use buildings on three sides. It also includes a prominent brick entrance fronting the highway.

- (iv) *Picnic area.* This amenity is generally 2,500 to 10,000 square feet in size and must include at least six tables with accompanying seating.



- (v) *Playground.* This amenity must be at least 3,000 square feet in size and must include at least five pieces of recreational equipment suitable for children, with at least one ADA compliant piece of equipment.

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- (vi) *Clubhouse*. This amenity is appropriate for mixed-use and residential projects, and must be at least 1,500 square feet in size, at least one and one-half stories, and include meeting space, recreational space, and restrooms.



Illustration - Clubhouse, pool, and playing courts at Paynter's Mill (Milton, DE)

- (vii) *Swimming pool*. This amenity is appropriate for mixed-use and residential projects, and must also include accompanying restrooms and changing areas.
- (viii) *Fitness center*. This amenity may be located inside a clubhouse (must be in addition to the square footage requirement for the clubhouse), and must include at least 15 pieces of equipment.
- (ix) *Pet park*. This amenity is appropriate for mixed-use and residential projects. It must be a minimum of 10,000 square feet in size and shall be enclosed with a fence made of materials and a design the department of planning and inspections determines will withstand the weather and climate conditions common to the town and will be able to contain dogs of various sizes and breeds.
- (x) *Developer's option*. Other forms of common amenity space which meets the standards of this section may be permitted if approved by the planning and zoning commission.
- (xi) *Site furniture standards*.
 - (a) *Benches*. Benches installed in common amenity spaces shall be black, six feet wide, Keystone Ridge Lamplighter (Model L16) as pictured below, or an approved alternative.



- (b) *Trash receptacles.* Trash receptacles installed in common amenity spaces shall be black, 32-gallon Keystone Ridge Midtown (Model MT3-32) as pictured below, or an approved alternative.



m. *Sign design.*

- (1) *Intent.* Signage along the U.S. Route 13 Corridor shall be designed and installed in such a way that it becomes a cohesive part of site design, improving the overall visual quality and pedestrian scale of U.S. Route 13. All signage shall be consistent with and complimentary to the architectural design and materials of the building it serves. The sign design regulations outlined herein shall be in addition to those sign regulations outlined in subsection 6.15 (Signs) of this Zoning Code. With respect to the CCM district, any inconsistencies between these regulations and those in subsection 6.15 (Signs) shall be resolved in favor of these regulations.
- (2) *Monument sign standards.*
 - (a) Monument signs shall mean any stand-alone, self-supporting structure affixed to a base that is equal to or wider than the sign itself.
 - (b) The height of the monument sign shall be no taller than eight feet in height, and the area of the monument sign shall be no greater than 75 square feet. A monument sign located on a property with one or more multi-tenant building shall have a height not to exceed 16 feet and an area not to exceed 125 square feet.
 - (c) The minimum monument sign setback is five feet from the existing right-of-way.
 - (d) Monument signs shall be designed and constructed utilizing the same building materials used in the construction of the associated building and/or site.

- (e) Monument signs may be externally or internally illuminated; external illumination must be directed downward.
- (f) Only one monument sign per street frontage shall be permitted on a property. A property with more than one street frontage may have up to a maximum of two monument signs, provided that the collective sign area of the two monument signs shall not exceed 150 percent of the maximum area permitted for one monument sign.



(3) *Wall signs.*

- (a) Wall signs shall not project above the roof, parapet or exterior wall.
- (b) Wall signs shall be appropriately scaled to the building. Signable area for wall signs shall be no greater than 150 square feet. An increase may be granted for those wall signs that are painted onto the building and are incorporated as a major element of the facade.
- (c) In multi-occupant, multi-story buildings, walls signs for first floor occupants shall not extend above the window sill of the second-floor windows.
- (d) Wall signs are encouraged to compliment the architectural style of the building.



(4) *Window signs.*

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- (a) Window signs shall not cover more than 50 percent of the window area on the frontage of a building or tenant space and shall be comprised of individual letters, logos or design elements that are not encompassed by a solid opaque background so as not to obscure the view through the window.
 - (b) Window signs are permitted on the first floor of a building only.



(5) *Projecting signs.*

- (a) Projecting signs shall project no more than 18 inches from a building face or wall.
- (b) Projecting signs shall not exceed 30 square feet for all faces.
- (c) Projecting signs shall have a minimum of eight feet of clearance from the ground, a maximum height of 25 feet, measured to the top of the sign, and shall not extend beyond the roofline.
- (d) Projecting signs shall not extend more than five feet from the building face nor extend beyond the curb line of any street or parking area.
- (e) Multi-tenant buildings are permitted one projecting sign per tenant. Only those tenants with ground floor occupancy are permitted to install a projecting sign.
- (f) Projecting signs are encouraged to compliment the architectural style of the building.



(6) *Awning signs.*

- (a) Awning signs are permitted on the ground floor of a building only.
- (b) Awning signs shall have a minimum of eight feet clearance from the ground.



(7) *Prohibited signs.*

- (a) Temporary or portable signs with changeable letter and numbers and portable trailer signs with changeable text panels.
- (b) Digital electronic signs of any kind with a sign area exceeding 25 square feet. For permitted digital electronic signs, the message displayed shall be static and shall not change more frequently than once every 15 minutes. The digital electronic sign shall not be located within 75 feet of any residential use, as measured from the sign to the closest point on the property line of the residential use.
- (c) Flashing signs.
- (d) Animated or revolving signs.
- (e) Signs that produce noise, sounds, emit smoke or vapor, particles or odors.
- (f) Signs with intermittent lights resembling or seeming to resemble the flashing lights customarily associated with emergency responder vehicles.
- (g) Vehicles and trailers used primarily as a sign.
- (h) Neon signs.

n. *Site design standards.*

- (1) Sites shall be developed and generally laid out in a block or grid pattern formed by an interconnected network of streets and alleys. Blocks are encouraged to range in size from approximately 400 to 700 feet in length. Blocks larger than 700 feet shall have mid-point pedestrian connections. Dead end cul-de-sac streets are prohibited.
- (2) Buildings are encouraged to directly face the street or a public green, square, or courtyard. Those dwellings and buildings that directly front a green, square or courtyard shall directly abut a sidewalk.

- (3) Higher-density multi-story commercial, office, hotels, and mixed-use buildings are encouraged to front directly on U.S. Route 13 near the primary street entrance(s). Townhouses, apartment buildings, and big box and grocery stores (which require larger parking areas) shall be located in the interior or rear of the site(s). Townhouses and apartment buildings shall be prohibited from directly fronting U.S. Route 13.
- (4) Connections and/or street stubs shall be made to adjacent properties to provide interconnectivity between other developed or potentially developable residential neighborhoods and commercial centers. Connections shall be made to all adjacent streets and roadways wherever practicable.
- (5) A street hierarchy shall be utilized to ensure that street and ROW widths are appropriately scaled based upon their functional classification. Commercial and mixed-use buildings shall front on wider boulevard streets (with or without a landscaped median) while apartment buildings and townhouse blocks are encouraged to be located on narrower neighborhood-type streets.
- (6) On-street parking is strongly encouraged where practical on both commercially-oriented boulevard-type streets and residentially-oriented neighborhood-type streets. Curb extensions shall be installed where practical while marked crosswalks shall be required at all intersections.
- (7) A bus stop with a corresponding bus shelter shall be provided, at a location approved by DART, wherever practical based on the proximity and condition of nearby bus stops.



Illustration - Darley Green mixed-use project along U.S. Route 13 in Claymont, DE with commercial and mixed-use buildings along the highway, followed by larger apartment buildings immediately behind the entrance fronting a central boulevard, and with smaller apartment and townhouse dwellings to the rear and periphery of the project.

o. *Residential use standards.*

- (1) *One- and Two-Family Dwellings.* One-Family (also known as Single-Family Detached) and Two-Family Dwellings shall be permitted on those parcels 0.25 acres or smaller in size, which are of record in the Office of the Recorder of Deeds as of November 1, 2022. Any such residential uses approved by the Town Council may front directly on U.S. Route 13.
- (2) *Townhouses Dwellings.* Townhouse dwellings shall follow the bulks standards (i.e. setbacks, height, lot coverage, etc.) and design standards set forth in the Planned Village Conditional use option in Section 6.14(1)F(3) of the Smyrna Zoning Ordinance.
- (3) *Apartment buildings.*
 - (a) Apartment buildings shall be two to four stories in height.
 - (b) Apartment buildings may not exceed 36 units in size. Projects with 48 to 96 dwelling units shall have a minimum of two apartment building sizes. Projects with 97 to 192 dwelling units shall have three apartment building sizes. Projects with 193 or more dwelling units shall be required to have at least four apartment building sizes.
 - (c) Apartment buildings shall be designed to resemble either: a block of attached (townhouse) dwellings, or a multi-story mixed-use or commercial building in the historic downtown of Smyrna or another historic downtown in Delaware.
 - (d) Off-street parking areas serving apartment dwellings shall be located to the rear of said building(s) or to the side of said building if the configuration of the development makes rear parking impractical. Apartment dwellings may also be served by attached or detached garages or carports along rear-access alleys.
 - (e) An unenclosed covered porch or stoop, at least four feet by four feet in size, shall be located at every ground floor exterior entrance in each apartment building.
 - (f) A concrete sidewalk at least four feet in width must extend from the street to every exterior ground floor entrance of every apartment building.

(Ord. No. 017-22, § 1, 10-17-22)



Illustration - 12-unit 3-story apartment building at Florin Hill (Mount Joy - Lancaster County, PA)



Illustration - Various sized apartment buildings (6 - 15 units in size) on a traditional grid street neighborhood with rear-accessed garages and parking areas at the Veranda of Norton Commons (Louisville, KY)



Illustration - Three-story 24-unit apartment building at Merchants Square at Spotsylvania Courthouse, VA



Illustration - Four-story 36-unit apartment building at Jefferson Pointe in West Chester, PA

p. *Definitions.*

Arcade: A continuous covered passageway parallel to and open to a street, open space or building and accessible and open to the public.

Awning: A roof-like cover that is temporary in nature and projects from the wall of a building for the purpose of shielding a doorway or window from the elements; also called a canopy

Balustrade: An entire railing system, including a top rail and its balusters and often a bottom rail, found along the edge of a porch, balcony or roof deck.

Belfry: That part of a steeple or top of a tower in which one or more bells are hung; also called a bell tower.

Belt course: A horizontal band of masonry extending horizontally across the facade of a building and occasionally encircling the entire perimeter; usually projects beyond the face of the building.

Cornice: Any molded horizontal projection that crowns or finishes the top of a wall where it meets the edge of a roof; the exterior trim of a structure where the wall and roof meet.

Cupola: A small roof tower, usually rising from the roof ridge.

Decorative window lintel: A decorative horizontal beam that spans the top of a window opening.

Dormer: A projection from a sloping roof that contains a window.

Marquee: A permanent canopy, usually of metal and glass, that projects over an entrance to a building, especially that of a theater or hotel.

Parapet: A decorative low wall or similar barrier at the edge of a roof, balcony, or terrace.

Pergola: A structure of open construction supported by regularly spaced posts or columns, often latticed and covered by climbing plants such as vines or roses.

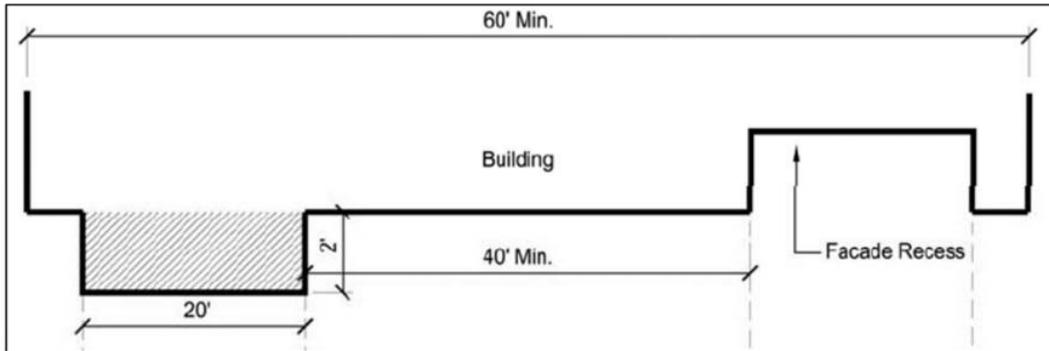
Pilaster: A column partially embedded in a wall and usually non-structural.

Porch: A roofed, open area, attached to or part of a building, and with direct access to or from it, providing shelter to a building entrance.

Portico: A covered entrance having a roof supported by a series of columns or piers, commonly at the front entrance to a building.

Turret: A cylindrical tower that is usually corbelled from one corner of the structure.

Wall offset: A projection or recess of at least two feet in depth. Wall offsets that are incorporated onto buildings having a length of at least 60 feet or greater shall be integrated at a minimum of every 40 feet, each offset shall have a minimum length of 20 feet.



- q. *Land use.* Land uses permitted in the corridor commercial (CCM) zoning district shall be those uses outlined in the land use table located below. Any use not listed herein shall be expressly prohibited.

Land Use	CCM Zoning District
Industrial, Manufacturing, Processing and Wholesaling	
Contractor Yards and Lumber Yards	CU
Flammable Liquid, Gas, and Bulk Fuel - Storage and Sale	CU
Grain Storage and Feed Mills	—
Junkyard	—
Manufacturing and Processing	CU
Machine or Metal Working Shops	—
Mini-storage Warehousing	SE 1
Research and Development Uses	SE
Transportation or Trucking Yards	—
Warehousing and Distribution	P 2
Wholesaling Beer and Alcohol Distributor	CU
Vehicle Towing/Impound Yard	CU
Recreation, Education and Assembly	
Go-kart, Miniature Automobile Racing	CU
Commercial Recreation Facilities, Indoor	P
Commercial Recreation Facilities, Outdoor	CU
Libraries, Museums	P
Meeting Facilities, Public or Private or Religious	P
Schools - Public and Charter	CU
Schools - Private	CU
Theaters	P
Universities or Colleges	CU
Trade Schools	CU
Residential₄	
One-Family Dwelling (Single-Family Detached)	P
Two-Family Dwelling	P
Accessory Dwelling Units (subject to the conditions for ADUs in 6.14)	CU 5
Dwelling: Townhouses	P 5
Dwelling: Apartments	P 5
Nursing Homes	P
Residence for Owner, caretaker or manager	P 5
Retail Trade	
Adult Entertainment Uses	—
Bars/Taverns	P
Body Art Establishments	CU
Breweries, Distilleries, and Wineries	P
Drive-through Retail	P
Drive-through Service	P
Farmers Markets	P
Flea Markets	SE
General Retail Business	P
Grocery and Big Box Department/Service Stores	P

Restaurant or Café	P
Services	
Bed and Breakfast Establishment	P
Commercial Services	CU
Crematorium	—
Day Care Center	P
Dry-cleaning, processing	P
Equipment Rental Yard	SE
Funeral Homes	P
General Services	P
Hospital	P
Kennel, Animal Boarding	SE
Lodging and Hotels	P
Offices	P
Personal Services	P
Public and Government Buildings	P
Emergency Services	CU
Veterinary Clinic/Hospital	P
Telecommunication Facilities	
Communication Antennas and Cell Towers	CU
Transportation and Infrastructure	
Passenger Transportation Facilities	CU
Vehicle Sales and Service	
Automobile Service Station and Convenience Store	P
Automobile and Trailer Rental	SE
Automobile/Vehicle/Boat/Farm Equipment Sales and Service, New and Used	SE
Automotive/Vehicle Repair Garages - Minor	P
Automotive/Vehicle Repair Garages - Major	SE
Car Washes	SE
Recreational Vehicles, Sales and Service	SE
End Notes	
¹ Only permitted on lots that do not have highway frontage or behind existing/new commercial uses	
² Only permitted when incidental to permitted use and any outdoor storage be screened from public view	
³ This use shall be screened. See section 8(c) (Street design standards) for street wall/buffer requirements	
⁴ Residential uses and residential properties listed on the National Historic Registry existing prior to the effective date of this zoning ordinance are considered legal, conforming uses.	
⁵ Residential uses are only allowed as part of a mixed-use development located above or behind the primary commercial use.	
Key	
P	Permitted
CU	Conditional Use
—	Use Not Permitted
SE	Special Exception

(Ord. No. 017-21, § 2, 11-1-21; Ord. No. 017-22, § 2, 10-17-22; Ord. No. 018-22, § 9, 10-17-22)

Editor's note(s)—Ord. No. 017-21, §§ 1, 2, adopted November 1, 2021, repealed the former §§ 5.21 and 5.22, and enacted a new § 5.21 as set out herein. The former § 5.21 pertained to North Corridor (NC) and derived from Ord. No. 008-14, § 1, 5-7-14; Ord. No. 021-14, § 4, 1-20-15; Ord. No. 003-16, § 5, 3-21-16; Ord. No. 009-19, §§ 2, 4, 5, 12-2-19; and Ord. No. 016-20, § 1, 7-20-20. The former § 5.22 pertained to South Corridor (SCZ) and derived from Ord. No. 015-14, § 1, 11-3-14; Ord. No. 003-16, § 6, 3-21-16; Ord. No. 009-19, §§ 3, 6, 7, 12-2-19; and Ord. No. 016-20, § 1, 7-20-20.